

Wade Park Avenue

December 18, 2025

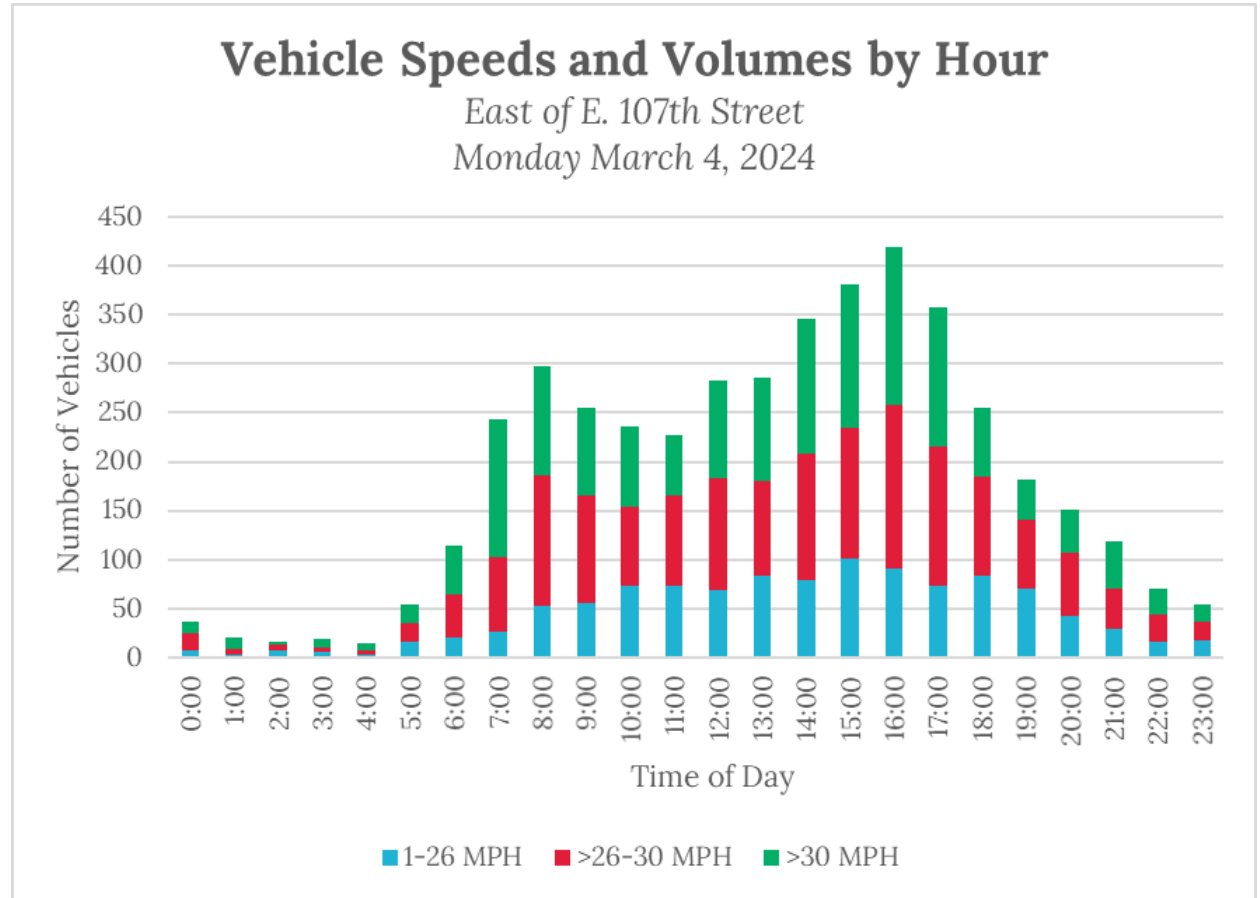
We've heard so *much* about speeding and dangerous driving

- Vision Zero – 2022
- Cleveland Moves – 2025
- Polling: 75% of Clevelanders agree that The City should invest more resources in improving biking, walking, and public transit
- Speed tables



... and the data agrees

- Over two thirds (68%) of vehicles are traveling over the 25 MPH speed limit at the eastern end of the street
- 4 serious injury crashes over the last 5 years
 - 112 total crashes in project limits during that time



Key community engagement themes

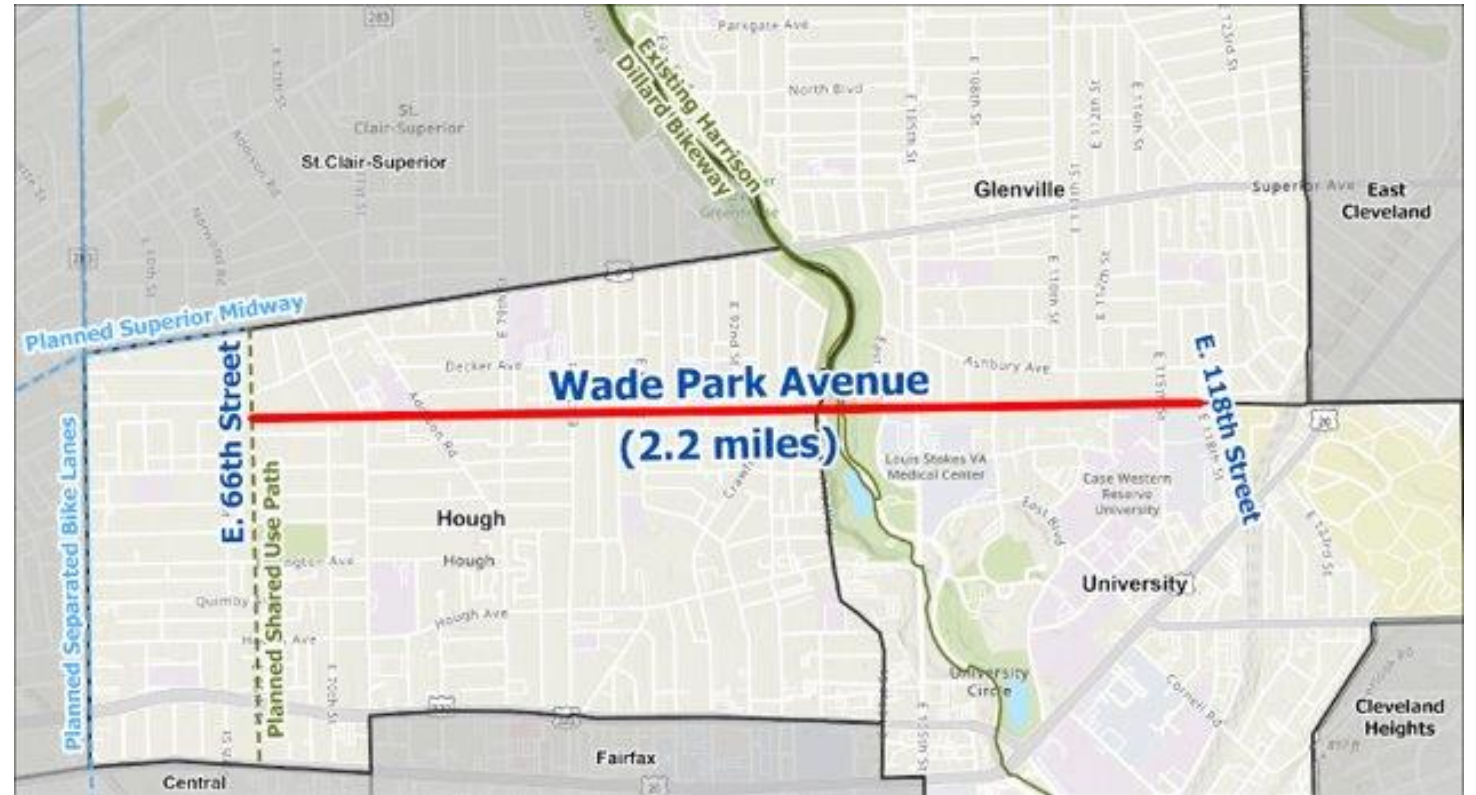
- More protected and high comfort infrastructure
- Sidewalks are in poor condition
- Driver behavior and vehicle speed
- Concerns around personal safety

- People want:
 - More speed tables and raised crossings
 - Traffic calming
 - Trees and shade
 - Better ADA accessibility
 - Better street maintenance
 - Streetlights that work
 - Secure bike parking



We have an opportunity to improve safety and quality of life

- Wade Park Avenue from E. 66th Street to E. 118th Street is due to be repaved
- We also want to respond to feedback by improving safety and mobility along the street



What we hope to accomplish

- Repave the street
- Upgrade curb ramps and replace broken sidewalks
- Reduce speeding by replacing overnight parking lanes with separated bike lanes
- Reduce crashes by replacing several traffic signals with mini roundabouts
- Add new crosswalks in key locations
- Add shade and reduce urban heat island effects by adding street trees and other greening



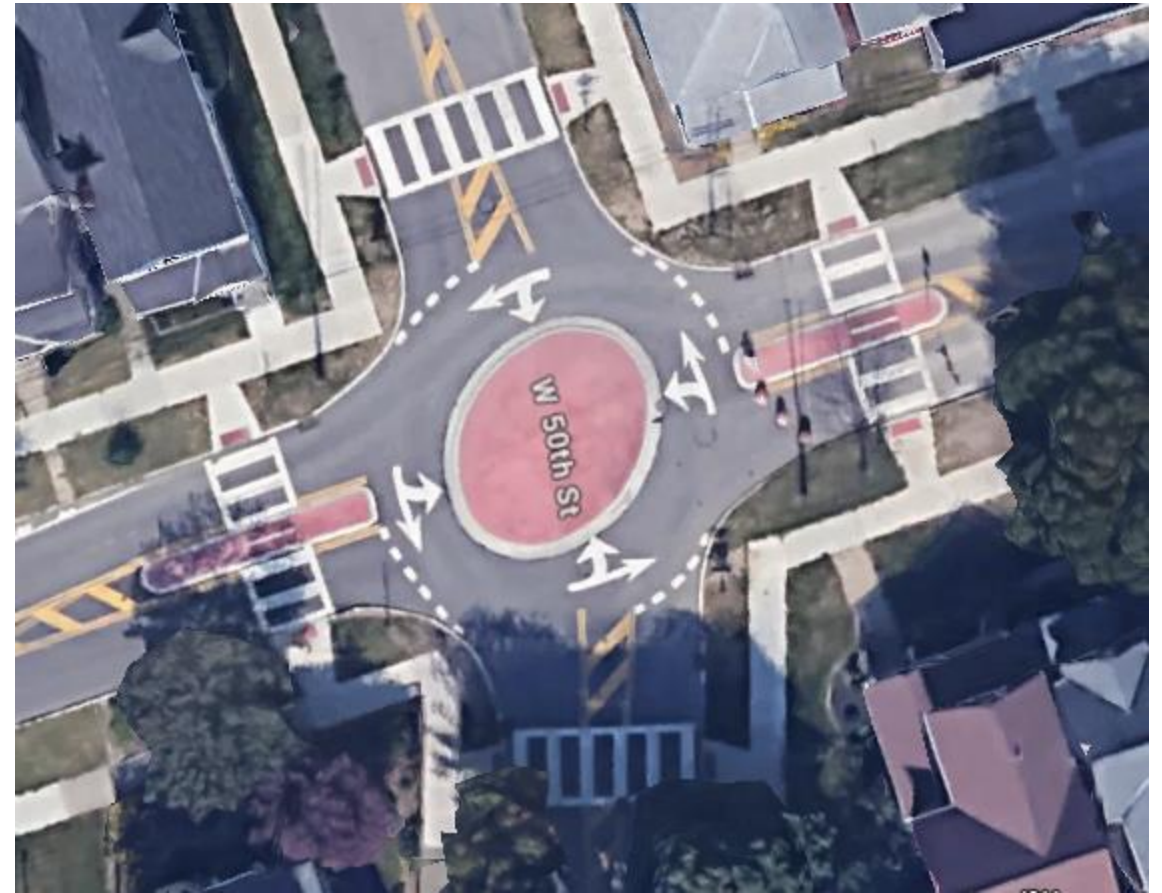
What is a mini-roundabout?

- Slows traffic speeds and increases driver awareness
- Reduces idling and crashes
- Large trucks and buses can drive over the middle if needed
- Proposed Roundabout Locations:
 - E. 71st
 - E. 82nd
 - Ansel Rd.
 - East Blvd.
 - E. 108th
 - E. 115th
- In total, 11 traffic signals will be replaced with 4-way stop signs or roundabouts

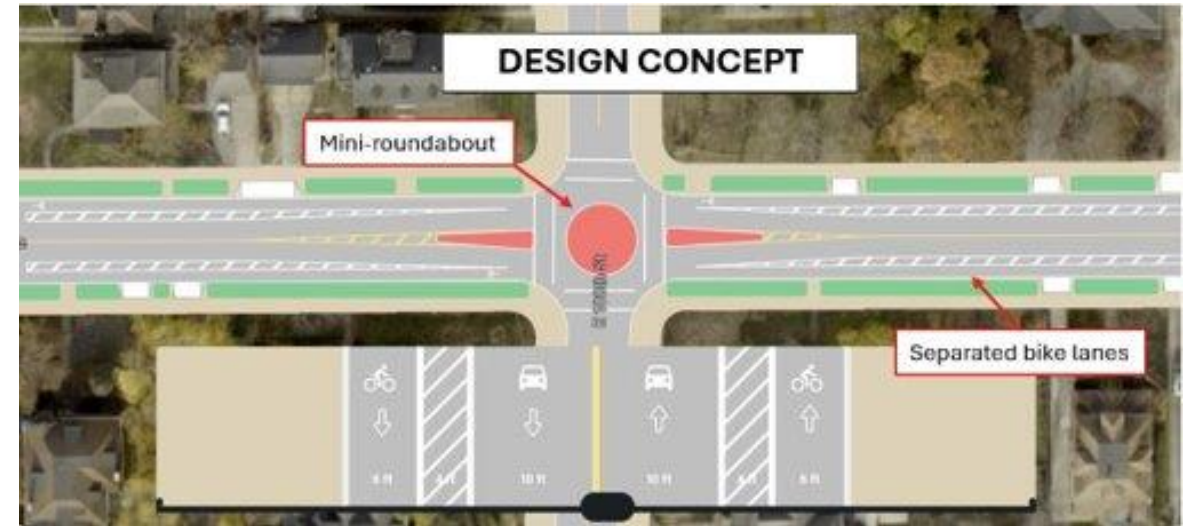


Why roundabouts?

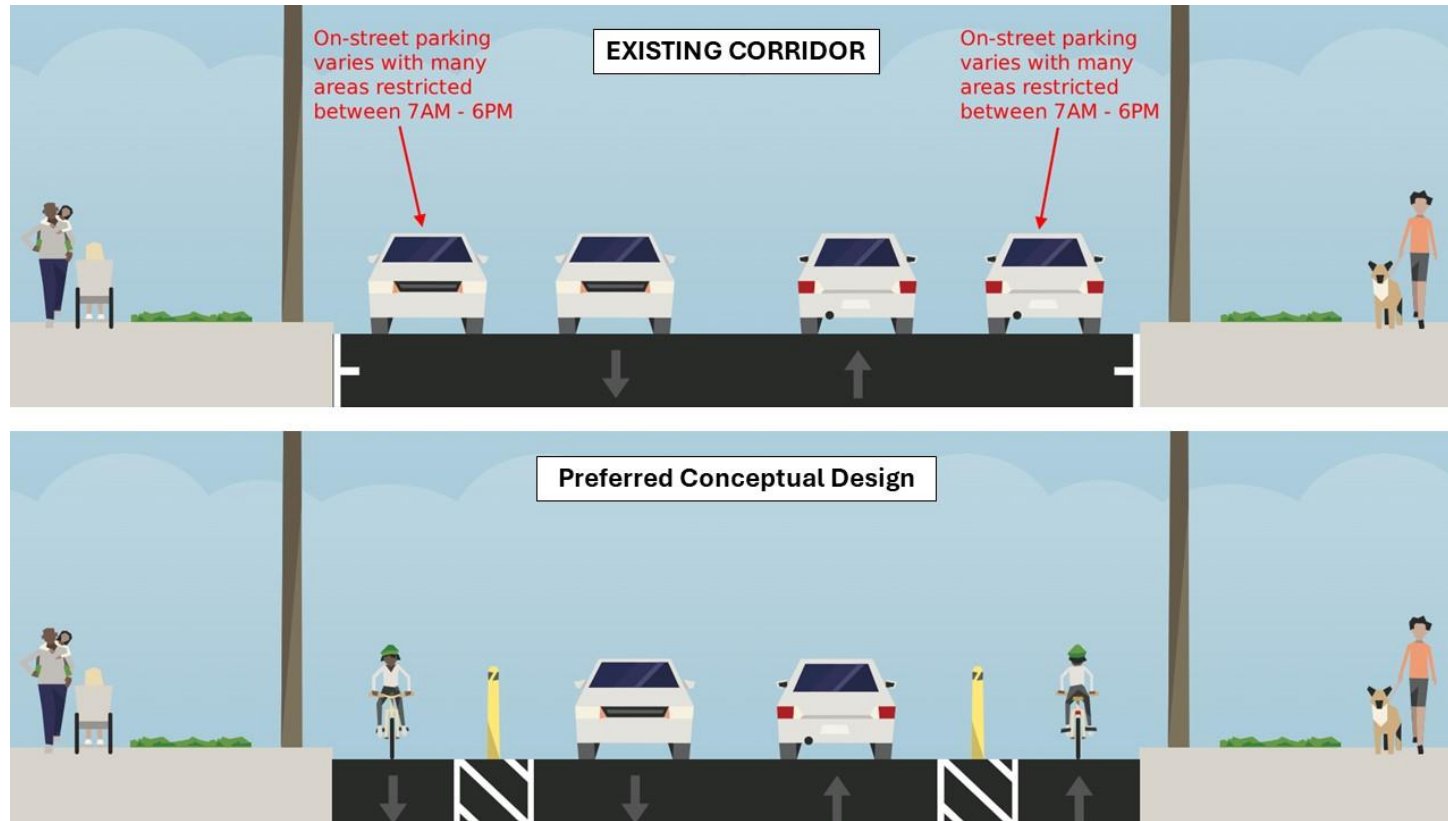
- Over the last 4 years, there were 112 crashes on Wade Park Avenue
- Nearly 76% of those crashes occurred at intersections (“angle crash”)
- Roundabouts have reduced total crashes by 60-80% in other locations in Cleveland



We can add mini-roundabouts on Wade Park without expanding the street



Adding bike lanes is proven to slow driver speeds and reduce crashes



What is a separated bike lane?

- Uses striping and vertical separation to organize the street
- Separated bike lanes:
 - Reduce average maximum vehicle speed by 28%
 - Reduce crashes for all modes by 30-50%
 - Reduce crossing distances for pedestrians



Why separated bike lanes?

- Physical separation hardens the edge of the travel lane, eliminating passing on the right
- Reduces crossing distances for pedestrians – less space to watch out for cars
- Increases visibility near driveways and side streets
- Increases space between moving vehicles and people using the sidewalk or in their front yards
- Creates safe routes for children and others to access schools and destinations without needing a vehicle – great for families and access to opportunity
- Empty parking lanes act as wide travel lanes that encourage fast driving
 - Parking for special events versus 24/7 traffic safety



We're focused on high comfort bikeways





I like the separated bike lanes.
You would feel so much safer walking on a main street. Or even driving. Because they [drivers] just whip around you and go in a bike lane.



- Killingsworth Meeting Place Community Conversation participant

We're building a network that connects Clevelanders to opportunities



We're thinking about winter maintenance



What's next?

We want to hear from you!

- Visit our project website
- Take our survey:



- **Design:**
July 2025 to August 2026
- **Construction:**
May 2027 to August 2028



Thank you



www.clevelandohio.gov/transportation-mobility